

Republic of the Philippines  
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS  
**OFFICE OF THE SECRETARY**  
Manila

097.13 DPWH  
03-01-2016

29 FEB 2016

NBC MEMORANDUM )

CIRCULAR NO. 02 )  
Series of 2016 02 of 16 )

TO : **ALL CITY/MUNICIPAL BUILDING OFFICIALS**  
**CITY/MUNICIPAL ENGINEERS ACTING AS BUILDING OFFICIALS**

SUBJECT : **Space Requirements for Loading/Unloading in the Development of Shopping Malls, Markets and Similar Facilities along National and Local Roads**


In line with the policy of the State to safeguard life, health, property and public welfare and pursuant to the power and authority vested in the DPWH Secretary under Section 203 of the National Building Code of the Philippines (PD 1096), you are hereby directed to strictly implement the following policies/standards, particularly to the Owners/Developers of Shopping Malls, Markets and similar facilities along national and local roads that have very high volume of pedestrians and vehicular traffic:

1. Minimum loading/unloading bay requirements {i.e. minimum off-street/off-Road Right-of-Way (RROW) cum on-site requirements}, aside from parking slot requirements per floor/lot area must be provided within their development as prescribed under **Rule VII, Section 707, Item 4 – Parking Area and Loading/Unloading Requirements, Table VII.4, Item 5 – Group E, Sub-Item 5.2 Division E-2, Revised Implementing Rules and Regulations (IRR) of PD 1096** (See Annex "A").
2. Pursuant to **Rule VII, Section 707, Item 4, Sub-Item e (iii), Revised IRR of PD 1096**, should the shopping malls, markets and similar facilities be located at major intersections or within 100.00 meters of such intersections, the distance between the street curb of the ingress/egress of such commercial lot/property and the straight curb of the intersection shall not be less than 50.00 meters (see Annex "B", Fig. VII-7).
3. Owners/Developers must provide loading/unloading bays within their property in order to prevent disturbance of traffic on national roads as well as on local roads. The site development plans shall include details and location of such bays and shall be included in the application requirements for Building Permits in your respective Office.

4. The minimum setbacks for shopping malls, markets and similar facilities along newly-developed thoroughfares shall conform to the values as prescribed under **Rule VIII, Section 804, Table VIII.3, Revised IRR of PD 1096** (See Annex "C") unless bigger setbacks are required by Zoning Ordinance in your areas of jurisdiction to provide adequate space for loading/unloading bays within the property of the owners. The required setbacks shall be measured from the property line (e.g. front, sides, and rear) to the outermost /exterior face of the building (grade level). However, if the building design has projection/cantilever at the upper level/s, then such required setbacks shall be measured from the outermost cantilever face of the building (above grade) to the property lines.
5. Per Department of Public Works and Highways (DPWH) Department Order No. 56 s. 2016 (copy attached), the DPWH Regional Directors and District Engineers are already directed to monitor compliance of the aforesaid policies/standards along national roads including the corresponding submission of quarterly reports. Your Office shall monitor compliance only along local roads and submit quarterly reports to the undersigned.

It is however directed that the Notice of Non-Compliance of the said policies/standards both along national roads and local roads shall be issued by your Office to the Owners/Developers of Shopping Malls, Markets and similar facilities not having the required provisions for loading/unloading bays.

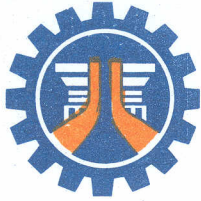
These policies and standard guidelines shall take effect after its publication once a week for three consecutive weeks in a newspaper of general circulation.

  
**ROGELIO L. SINGSON**  
Secretary

Department of Public Works and Highways  
Office of the Secretary  
  
WIN6R01251

Encl: a/s

5.1 MCDC/AMD/DBP



Republic of the Philippines  
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS  
**OFFICE OF THE SECRETARY**  
Manila

897.13 DPWH  
03-01-2016

29 FEB 2016

DEPARTMENT ORDER )  
NO. **56** )  
Series of 2016 )

**SUBJECT: Space Requirements for Loading/Unloading in the Development of Shopping Malls, Markets and Similar Facilities along National Roads**


In accordance with the Department's mandate to ensure effective implementation of the rules and regulations of the National Building Code of the Philippines (PD 1096) in the provision of efficient access to public transportation in commercial land developments, Owners/Developers of shopping malls, markets and similar facilities along national roads that have very high volume of pedestrians and vehicular traffic must strictly comply with the following policies/standards:

1. Minimum loading/unloading bay requirements {i.e., minimum off-street/off-Road Right-of-Way (RROW) cum on-site requirements}, aside from parking slot requirements per floor/lot area must be provided within their development as prescribed under **Rule VII, Section 707, Item 4 – Parking Area and Loading/Unloading Requirements, Table VII.4, Item 5 – Group E, Sub-Item 5.2 Division E-2, Revised Implementing Rules and Regulations (IRR) of PD 1096** (See Annex "A").
2. Pursuant to **Rule VII, Section 707, Item 4, Sub-Item e (iii), Revised IRR of PD 1096**, should the shopping malls, markets and similar facilities be located at major intersections or within 100.00 meters of such intersections, the distance between the street curb of the ingress/egress of such commercial lot/property and the straight curb of the intersection shall not be less than 50.00 meters (See Annex "B", Fig. VII-7).
3. Owners/Developers must provide loading/unloading bays within their property in order to prevent disturbance of traffic on national roads. The site development plans shall include details and location of such bays and shall be included in the application requirements for Building Permits subject for approval of Local Building Officials (LBOs).
4. The minimum setbacks for shopping malls, markets and similar facilities along newly-developed thoroughfares shall conform to the values as prescribed under **Rule VIII, Section 804, Table VIII.3, Revised IRR of PD 1096** (See Annex "C") unless bigger setbacks are required by zoning ordinance of local government units to provide adequate space for loading/unloading bays within the property of the owners. The required setbacks shall be measured from the property line

(e.g. front, rear, sides) to the outermost/exterior face of the building (at grade level). However, if the building design has projection/cantilever at the upper level/s, then such required setbacks shall be measured from the outermost cantilever face of the building (above grade level) to the property lines.

5. DPWH Regional Directors and District Engineers, in coordination with the concerned LBOs, shall monitor compliance of the aforesaid policies/standards along national roads and submit quarterly reports to the undersigned. Notice of non-compliance of the said policies/standards along national roads shall be issued by the concerned LBOs to the Owners/Developers of shopping malls, markets and similar facilities along national roads not having the required provisions for loading and unloading bays.

This Order shall take effect immediately.

  
**ROGELIO L. SINGSON**  
Secretary

Department of Public Works and Highways  
Office of the Secretary



WIN6R01251

5.1 DBP

## 4. Parking Slot, Parking Area and Loading/Unloading Space Requirements

- a. The parking slot, parking area and loading/unloading space requirements listed hereafter are generally the minimum off-street cum on-site requirements for specific uses/occupancies for buildings/structures, i.e., all to be located outside of the road right-of-way (RROW).
- b. The size of an average automobile (car) parking slot must be computed at 2.50 meters by 5.00 meters for perpendicular or diagonal parking and at 2.15 meters by 6.00 meters for parallel parking. A standard truck or bus parking/loading slot must be computed at a minimum of 3.60 meters by 12.00 meters. An articulated truck slot must be computed at a minimum of 3.60 meters by 18.00 meters which should be sufficient to accommodate a 12.00 meters container van or bulk carrier and a long/hooded prime mover. A jeepney or shuttle parking/loading/unloading slot must be computed at a minimum of 3.00 meters by 9.00 meters. The parking slots shall be drawn to scale and the total number of which shall be indicated on the plans and specified whether or not parking accommodations are attendant-managed.
- c. The parking space ratings listed below are minimum off-street/off-RROW cum on-site requirements for specific uses/occupancies for buildings/structures, i.e., all to be located outside of the road right-of-way (RROW):

**Table VII.4. Minimum Required Off-Street (Off-RROW) cum On-Site Parking Slot, Parking Area and Loading/Unloading Space Requirements by Allowed Use or Occupancy**

Specific Uses or of Occupancy (refer to Section 701 of this Rule)	Reference Uses or Character of Occupancies or Type of Buildings/Structures	Minimum Required Parking Slot, Parking Area and Loading Space Requirements
<b>1. GROUP A</b>		
1.1. Division A-1	Single family and multi-family dwelling units [whether single-detached (R-1), single-attached or duplex (R-2) or rowhouse (R-3)], each privately owned or lots with dwelling units located in residential subdivisions/developments regardless of number of hectares/dwelling units	Units with a lot measuring 32.00 to 72.00 sq. meters and/or with a dwelling unit having a gross floor area of from 18.00 to 22.00 sq. meters - a minimum of one (1) pooled off-street cum on-site parking slot* for every six (6) lots or lots with dwelling units;
		Units with a lot measuring 50.00 to 96.00 sq. meters and/or with a dwelling unit having a gross floor area of from 30.00 to 42.00 sq. meters - a minimum of one (1) pooled off-street cum on-site parking slot* for every four (4) lots or lots with dwelling units;
		<b>Note:</b> * The parking slot requirements shall be an integral part of buildings/structures and any parking slot provided outside the building/structure will be quantified only as buffer parking.



Specific Uses or of Occupancy (refer to Section 1.3 of this Rule)	Reference Uses or Character of Occupancies or Type of Buildings/Structures	Minimum Required Parking Slot, Parking Area and Loading Space Requirements
	Private hospital	One (1) off-street cum on-site car parking slot for every twelve (12) beds; and one (1) off-RROW (or off-street) passenger loading space that can accommodate two (2) queued jeepney/shuttle slots; provide at least one (12) loading slot for articulated truck or vehicle (a 12.00 meters long container van plus 6.00 meters length for a long/hooded prime mover) and one (1) loading slot for a standard truck for every 5,000.00 sq. meters of GFA; and provide truck maneuvering area outside of the RROW (within property or lot lines only)
4.3. Division D-3	Nursing homes for ambulatory patients, school and home, for children over kindergarten age, orphanages and the like	One (1) off-street cum on-site car parking slot for every twelve (12) beds; and one (1) off-RROW (or off-street) passenger loading space that can accommodate two (2) queued jeepney/shuttle slots
<b>5. GROUP E</b>		
5.1. Division E-1	Terminals, Inter-modals or Multi-modals, Depots and the like (UTS)	One (1) car slot for every 500.00 sq. meters of gross floor area or for a fraction thereof; and one (1) off-RROW (or off-street) passenger loading space that can accommodate two (2) queued jeepney/shuttle slots or two (2) queued bus slots whichever is applicable; maneuvering area of buses, trucks and like vehicles shall be outside of the RROW (within property or lot lines only)
	<p><i>Note:</i></p> <p><i>* The parking slot requirements shall be an integral part of buildings/structures and any parking slot provided outside the building/structure will be quantified only as buffer parking.</i></p>	

Specific Uses or of Occupancy (refer to Section 1.3 of this Rule)	Reference Uses or Character of Occupancies or Type of Buildings/Structures	Minimum Required Parking Slot, Parking Area and Loading Space Requirements
	Transit Stations and the like (UTS)	Provide on each side of the RROW one (1) off-RROW (or off-street) passenger loading space that can accommodate four (4) queued jeepney/shuttle slots or three (3) queued bus slots whichever is applicable; in case of elevated mass transit stations, on-roadway terminals or on-RROW terminals on both sides of the RROW may be considered
5.2. Division E-2	Neighborhood shopping center/supermarket (C-1)	One (1) car slot for every 100.00 sq. meters of shopping floor area
	Public Markets (C)	One (1) customer (buyer) jeepney/shuttle parking slot for every 150.00 sq. meters of wet and dry market floor area and one (1) vendor (seller) jeepney/shuttle parking slot or loading space for every 300.00 sq. meters of wet and dry market floor area; and one (1) off-RROW (off-street) terminal that can accommodate at least two (2) jeepneys and six (6) tricycles for every 1,000.00 sq. meters of wet and dry market floor area
	Restaurants, fast-food centers, bars and beerhouses (C)	One (1) car slot for every 30.00 sq. meters of customer area
	Nightclubs, super clubs and theater-restaurants (C)	One (1) car slot for every 20 sq. meters of customer area; and two (2) tourist parking slots for tourist bus parking slots for each theater-restaurant
	<p>Note:</p> <p>* The parking slot requirements shall be an integral part of buildings/structures and any parking slot provided outside the building/structure will be quantified only as buffer parking.</p>	

Specific Uses or of Occupancy (refer to Section 1.3 of this Rule)	Reference Uses or Character of Occupancies or Type of Buildings/Structures	Minimum Required Parking Slot, Parking Area and Loading Space Requirements
	Nightclubs, super clubs and theater-restaurants (C)	One (1) car slot for every 30.00 sq. meters of customer area
	Units located in office, commercial or mixed-use condominium buildings/structures regardless of number of storeys	Units with a gross floor area of from 18.00 to 40.00 sq. meters – provide one (1) pooled parking slot* for every two (2) units or for a fraction thereof;
		Unit with a gross floor area of from 41.00 to 70.00 sq. meters – provide one (1) parking slot* for each unit; and
		Unit with a gross floor area of more than 70.00 sq. meters provide one (1) parking slot* for every 70.00 sq. meters and for a fraction thereof;
	Columbarium	One (1) car slot for every compartment niche
5.3. Division E-3	Aircraft hangars, open parking carports and garages, etc.	One (1) car slot for every 1,000.00 sq. meters of gross floor area and one (1) bus slot for every one hundred (100) workers; if number of workers exceed two hundred (200), provide one (1) off-RROW (or off-street) passenger loading space that can accommodate two (2) queued jeepney/shuttle slots
<b>6. GROUP F</b>		
6.1. Division F-1	Industrial buildings, mills, breweries, etc. (I)	One (1) car slot for every 1,000.00 sq. meters of gross floor area and one (1) bus slot for every one hundred (100) workers; if number of workers exceed two hundred (200), provide one (1) off-RROW (or off-street) passenger loading space that can accommodate two (2) queued jeepney/shuttle slots; provide at least one (1) loading slot for
	<p><i>Note:</i></p> <p>* The parking slot requirements shall be an integral part of buildings/structures and any parking slot provided outside the building/structure will be quantified only as buffer parking.</p>	



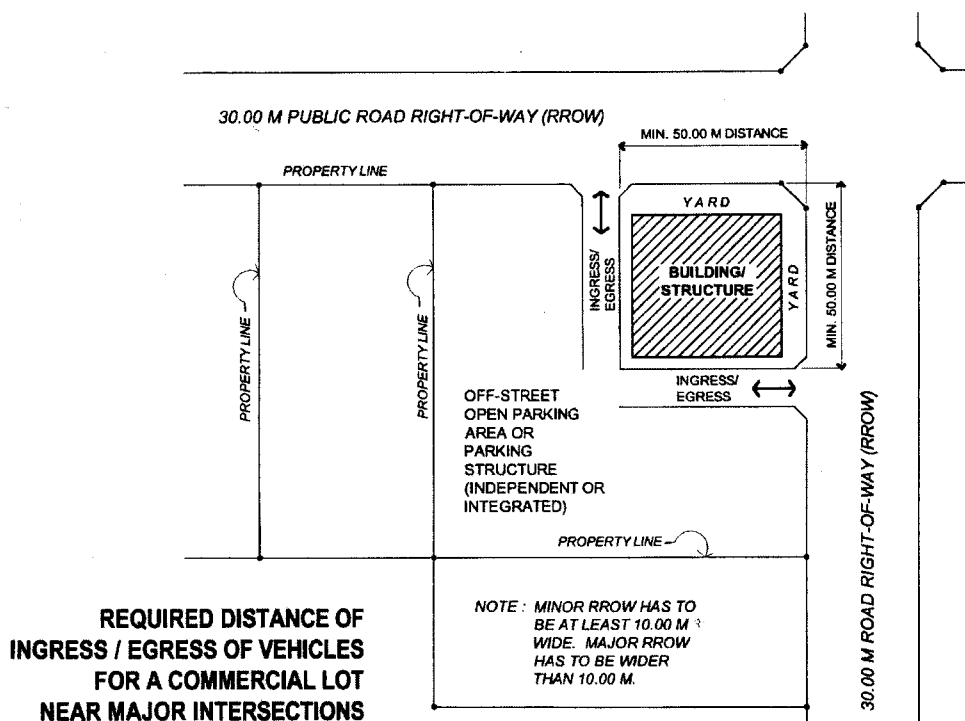


Fig.VII.7.

- iv. For R-2, R-3, GI, C, C-2 and C-3 uses or occupancies, front yards abutting RROW are not to be used for long-term off-street parking. Due to the very public nature of these uses (high vehicular and pedestrian concentrations), the front yard (a transition space between the RROW and the building/structure) shall be used exclusively for driveways, off-RROW loading spaces, short-term off-RROW parking and landscaping (hardscape and softscape) treatment. Temporary or short-term off-street parking, particularly on driveways, shall preferably be only for visitors to these buildings/structures.
- v. For Basic R-2 and Basic R-3 uses or occupancies (for single family dwelling units only), up to 50% of the front yard abutting the RROW may be paved/hardscaped, i.e., converted into a courtyard for carport use. Such use shall not be permitted in all other uses or occupancies.

## SECTION 708. Minimum Requirements for Group A Dwellings

### 1. Dwelling Location and Lot Occupancy

The dwelling shall occupy not more than 90% of a corner lot and 80% of an inside lot, and subject to the provisions on Easements of Light and View of the Civil Code of Philippines, shall be at least 2.00 meters from the property line.

### 2. Light and Ventilation

Every dwelling shall be so constructed and arranged as to provide adequate light and ventilation as provided under Section 805 to Section 811 of the Code.

### 3. Sanitation

Every dwelling shall be provided with at least one sanitary toilet and adequate washing and drainage facilities.

**SECTION 804. Sizes and Dimensions of Courts**

1. Minimum sizes of courts and yards and their least dimensions shall be governed by the use, type of construction, and height of the building as provided hereunder, provided that the minimum horizontal dimension of said courts and yards shall be not less than 2.00 meters. All inner courts shall be connected to a street or yard, either by a passageway with a minimum width of 1.20 meters or by a door through a room or rooms.
2. The required open space shall be located totally or distributed anywhere within the lot in such a manner as to provide maximum light and ventilation into the building. (Figures VIII.12. through VIII.15.)
3. YARD – the required open space left between the outermost face of the building/structure and the property lines, e.g., front, rear, right and left side yards. The width of the yard is the setback. Yards prescribed for Commercial, Industrial, Institutional and Recreational Buildings are shown in Table VIII.3. hereafter.

**Table VIII.2. Minimum Setbacks for Residential Buildings/Structures**

YARD	Type of Residential Use/ Occupancy						
	R – 1 (meters)	R - 2		R-3		R - 4 (individual lot/unit) (meters)	R - 5**** (meters)
		Basic (meters)	Maximum (meters)	Basic (meters)	Maximum (meters)		
Front	4.50	3.00	8.00 *	3.00	8.00 *	4.50	6.00
Side	2.00	2.00 **	2.00 **	***	2.00 (optional)	2.00 (optional)	3.00
Rear	2.00	2.00	2.00	***	2.00	2.00	3.00

**Notes:**

- a) The setback requirements in Table VIII.2. above are for newly-developed subdivisions.
- b) \* Total setback only at grade (or natural ground) level, i.e., 3.00 meters + 5.00 meters = 8.00 meters (to accommodate part of the minimum parking requirement outside the designated area for the front yard). The second and upper floors and mezzanine level shall thereafter comply with the minimum 3.00 meters setback unless otherwise provided under the Code.
- c) \*\* Setback required for only one (1) side. Setbacks on two sides shall be optional.
- d) \*\*\* Abutments on two sides and rear property lines may be allowed with conditions as enumerated under Section 804, Subsection 10 of this Rule.
- e) \*\*\*\* Mixed-Use Buildings/Structures in R-5 lots shall be considered a commercial use or occupancy if a substantial percentage, i.e., 55% of the Gross Floor Area (GFA) is commercial.
- f) In cases where yards/setbacks are impossible to attain or where frontage and depth of lots are similar to that of Open Market or Medium Cost Housing Projects, abutments on the sides and rear property lines may be allowed and 1.50 meters front yard is left open as transition area.

**Table VIII.3. Setbacks for Commercial\*, Industrial, Institutional and Recreational Buildings**

Road Right-of-Way (RROW) Width (meters)	Front (meters)	Side (meters)	Rear (meters)
30.00 & above	8.00	5.00	5.00
25.00 to 29.00	6.00	3.00	3.00
20.00 to 24.00	5.00	3.00	3.00
10.00 to 19.00	5.00	2.00	2.00
Below 10.00	5.00	2.00	2.00

**Note:**

- \* Mixed-Use Buildings/Structures in R-5 lots may be considered a commercial development if a substantial percentage of the GFA is commercial.